



## Providence Bicycle and Pedestrian Advisory Commission

Jorge O. Elorza, Mayor

### **Staff Report: King Street Conceptual Review – Olneyville – Ward 15 (For Action)**

*Presented at February 21, 2018 BPAC meeting*

#### **Project Background**

The City of Providence Department of Planning and Development seeks comments from the BPAC regarding green infrastructure and neighborhood bikeway improvements to King Street. This will be a concept level review of the project. When advanced in the future, this project will come back to the BPAC for preliminary plan review.

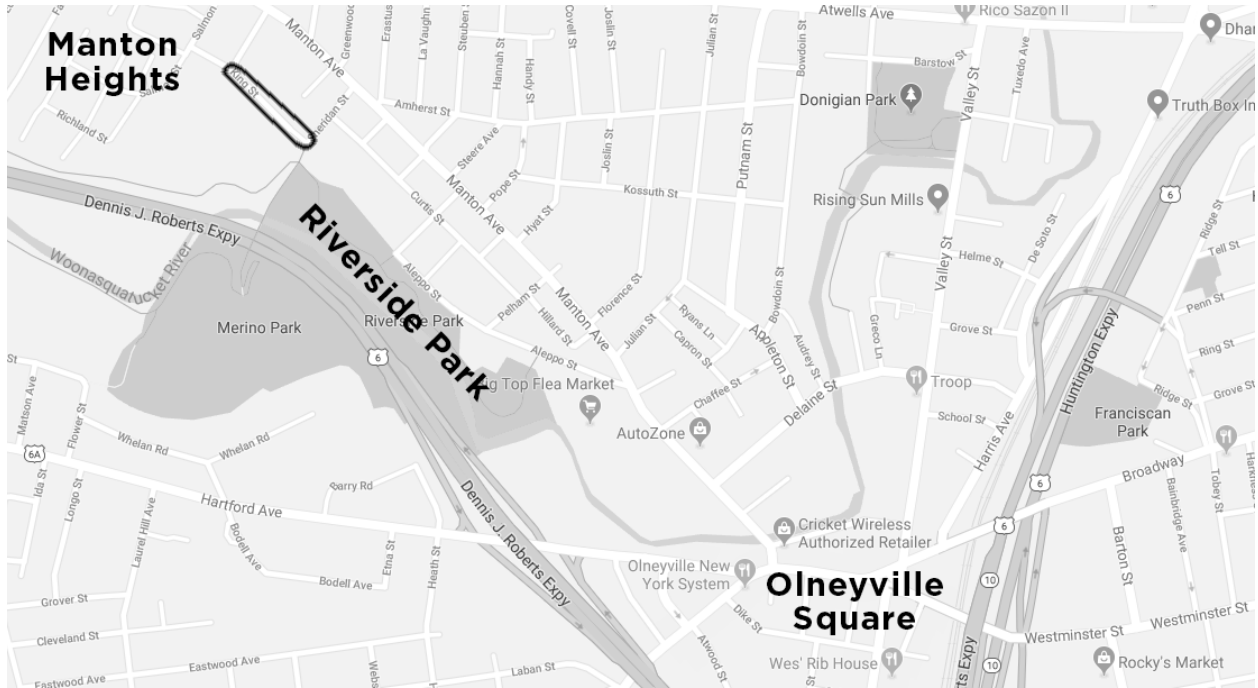
Riverside Park sees a large amount of runoff which erodes the hillside near the bridge over US-6 to Merino Park. This runoff is mostly from King Street, which runs parallel to Manton Ave and is currently blocked off to traffic due to prior issues with illegal dumping. Additionally, the City seeks to better connect low-income residents of Manton Heights with the resources of the Park and the Woonasquatucket Greenway that runs through it. These two demands and the context of new development that will be occurring on either side of King Street create an opportunity for both green infrastructure to mitigate runoff and the City's first neighborhood bikeway.

Neighborhood bikeways are streets with low motorized traffic volumes and speeds, designated and designed to give bicycle travel priority. Neighborhood bikeways use signs, pavement markings, and speed and volume management measures to discourage through trips by motor vehicles and create safe, convenient bicycle crossings of busy arterial streets. Neighborhood bikeways are an important tool cities can use to provide all-ages-and-abilities bicycle connections through low-traffic neighborhood streets without the need for extensive separation. For more information on neighborhood bikeways, otherwise known as bicycle boulevards, read about them in the NACTO urban bikeway design guide: <https://nacto.org/publication/urban-bikeway-design-guide/bicycle-boulevards/>



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**Project Map**



**Project Description**

The conceptual design includes the following:

- A parking land on the south side of the street broken up by bioretention basins contained within curb extensions.
- Two-way vehicular traffic (20 ft between northern curb and parking lane/bulb-outs)
- Two speed tables to reduce vehicular travel speeds
- Permeable pavement for the sidewalk on the south side of the street.

**Staff Recommendations to BPAC**

Staff to the BPAC make the following recommendations:

- At the intersections of King St with Dearborn St and Sheridan St, expand the width of the bulb-outs to create an 18 ft curb-to-curb distance. This narrowing encourages turning vehicles to



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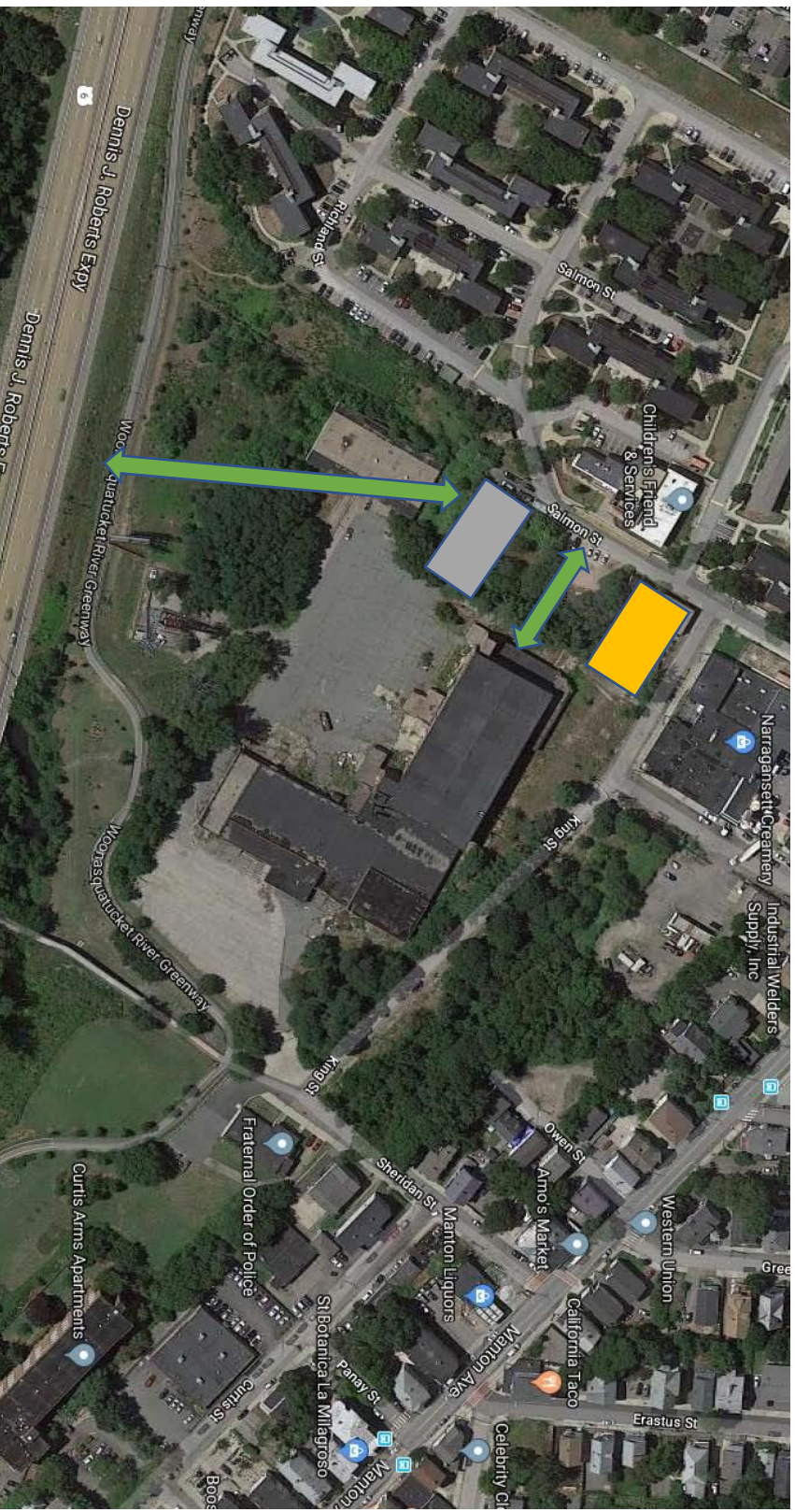
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enter the roadway at safe speeds. Large vehicles can still make the turns by utilizing the opposite lane in the anticipated low-traffic conditions.

- Include signage to both make clear that bicyclists and pedestrians have priority on the street and to direct road users to the Woonasquatucket Greenway, Riverside Park, and other destinations within a walkable or bikeable distance (e.g. Manton Ave, Manton Heights, Atwells Ave, or any other bike connections in the neighborhood)
- Move the western of the two speed tables to the intersection with Dearborn St, converting it into a raised crosswalk.
- In addition to the crosswalk across Sheridan St, install clear pavement markings showing bicyclists where to go. This is potentially an appropriate application of shared-lane markings (sharrows).
- Ensure that the design of the new development along the north side of King St is supportive in its design of active transportation.

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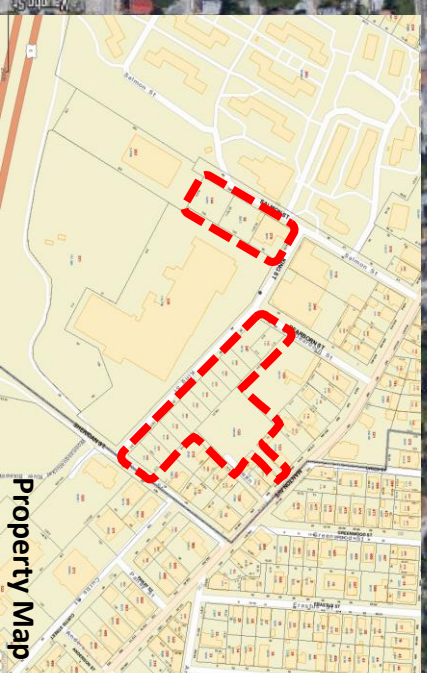
Respectfully submitted by Alex Ellis.



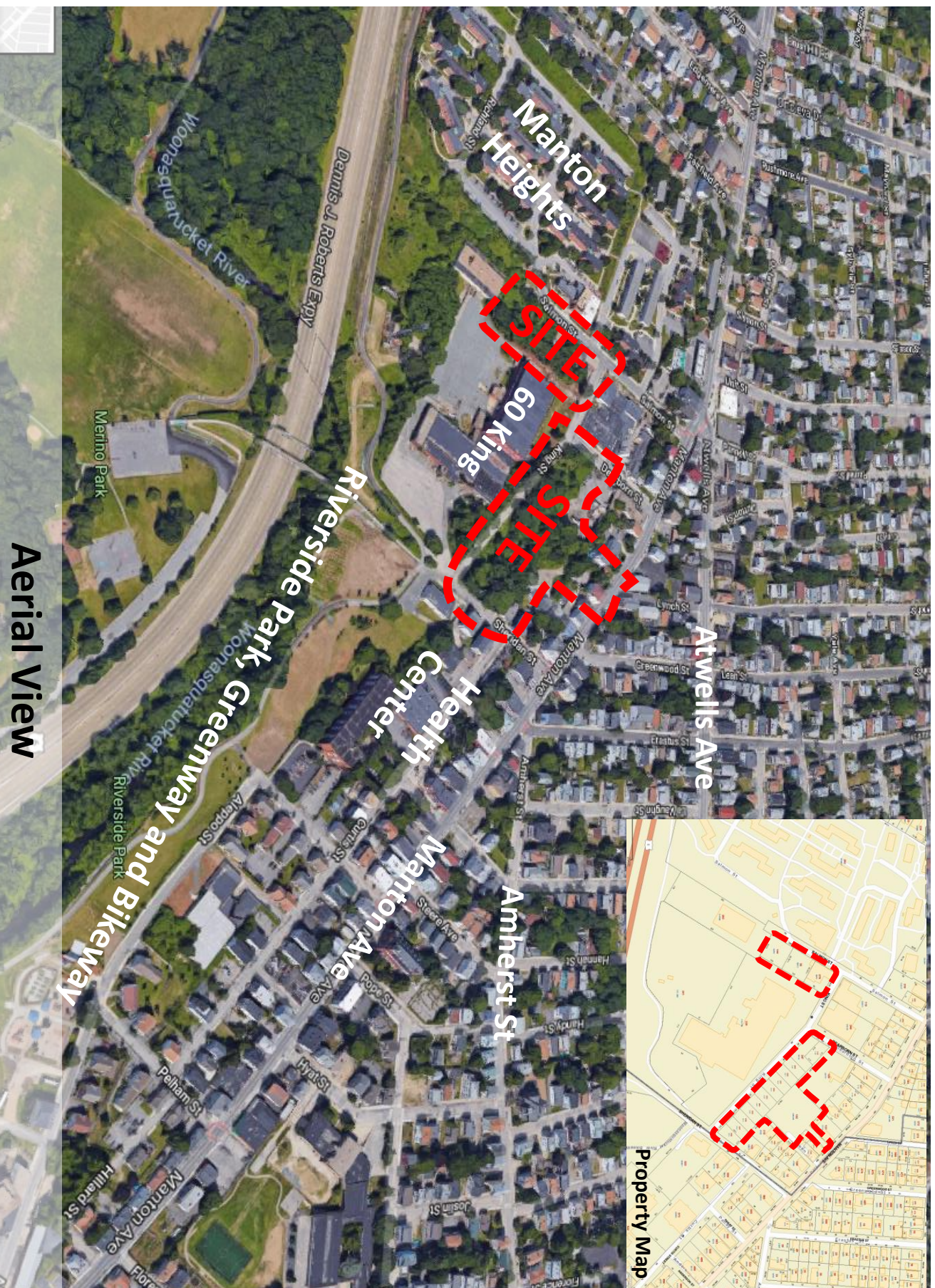
**GOALS:**

- Improve connection to WR Greenway from Salmon Street
  - Steep grade
  - Industrial building
- Integration of Manton Heights with new developments
  - Green space continuity from 60 King to Salmon Street
- Parking for PHA off Salmon Street
- Community space on Salmon and King Street corner
- Mindful of storm water run off





Property Map



Aerial View

3-45 King St, 72-74 King, 50-60 Salmon, 28-30 Dearborn, 370 Manton Ave







King Crossing: King Street Elevation



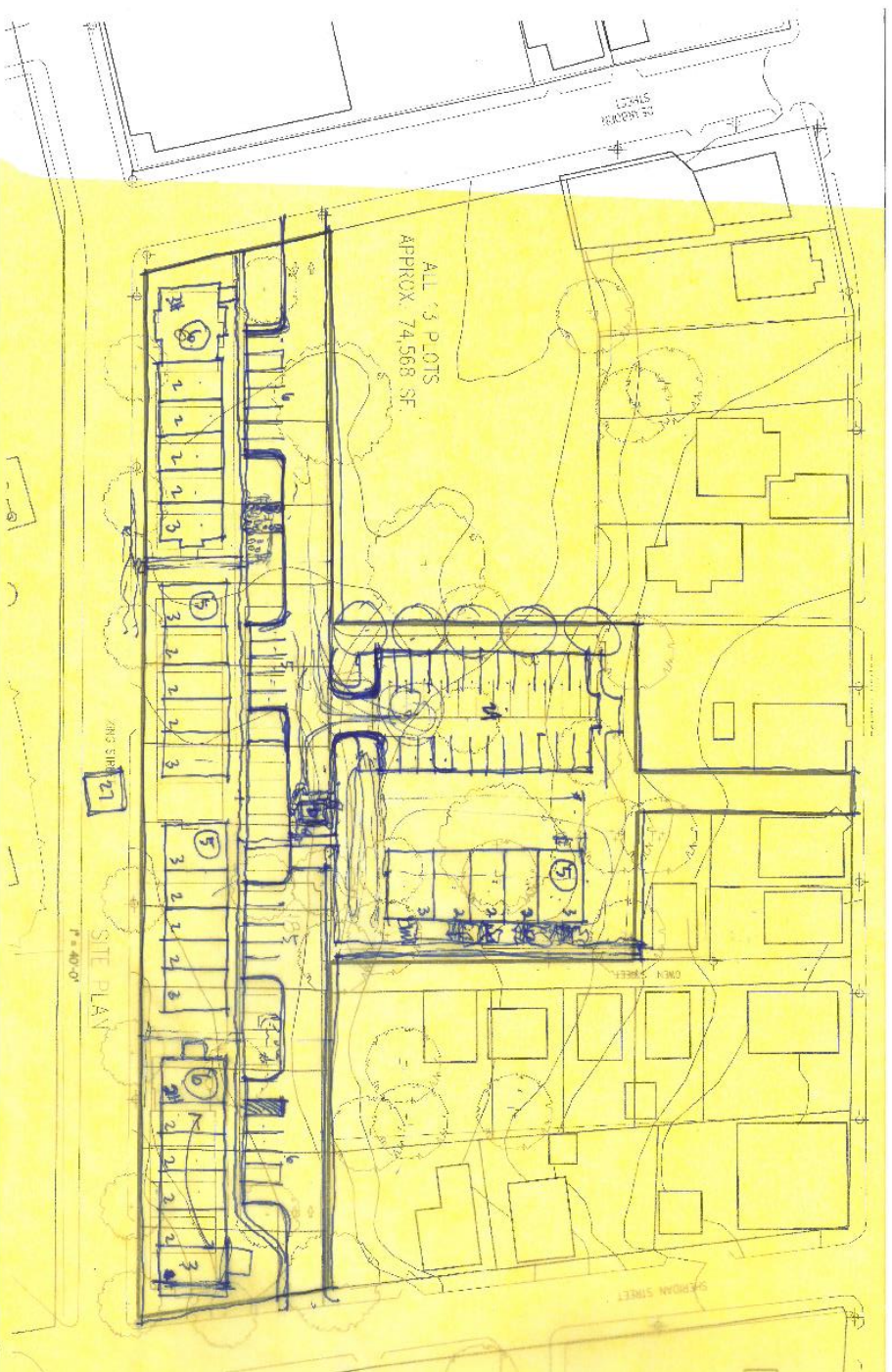
King Crossing: King Street Perspective



**3-45 King Street, 28-30 Dearborn,  
370 Manton Ave.**



# Durkee Brown alternative site map





No.

1.

DATE

DESCRIPTION

DESIGNER REVIEWER

xx/xx

xx

SEAL

SEAL

SCALE:

HORZ.: 1"=20'

VERT.:

DATUM:

HORZ.:

VERT.:

20

10

0

20

GRAPHIC SCALE

f

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CONCEPT 2

60 KING STREET

RHODE ISLAND

PROJ. NO.: 20141278A50

DATE: DECEMBER 2017

CS-102

The main site plan illustrates a street layout with King Street running horizontally. Dearborn Street intersects from the top, and Sheridan Street intersects from the bottom. The plan shows various engineering details including curbs, sidewalks, and green highlighted areas. A north arrow is located in the bottom right corner.

BMP TABLE		
ID	TYPE	TOTAL SURFACE AREA (SF)
A	BIORETENTION, CURB EXTENSION PLANTER – TYPE D	640
B	BIORETENTION, TRAVEL LANE ADJACENT PLANTER – TYPE B	490
C	TREE FILTER, PRETREATMENT BASIN – TYPE B	135

IMPERVIOUS AREAS	
PRE-DEVELOPMENT	18,445 SF
POST-DEVELOPMENT	20,185 SF